

### Existing Conditions

According to the most recent MOA geographic information system (GIS) files, the study area contains a diversity of land uses including commercial, residential, and industrial properties (see Figure 3.2120). Undeveloped land is being used for open space or utility corridors, or is vacant and not in use. Figure 3.2019 shows the percentages of the different land uses found in the study area. Most residential land occurs near the project termini. The industrial land is near the center of the proposed project. The Alaska Railroad ROW is used for railroad purposes. The road ROW is used by DOT&PF and MOA for roads.

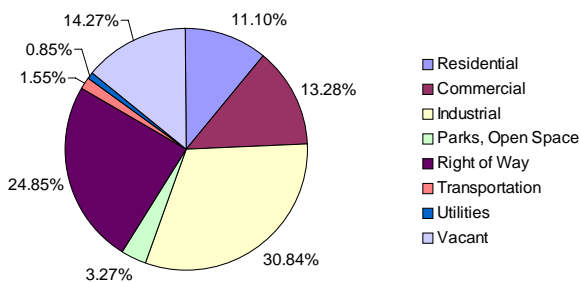


Figure 3.2019 Land Use in the Study Area

The Anchorage 2020 comprehensive plan (MOA 2001) identifies much of the study area as industrial reserve (see Figure 3.2221). An industrial reserve, as described in the comprehensive plan, contains large vacant areas zoned for industrial use and is strategically located near important transportation facilities such as the Ted Stevens Anchorage International Airport, the port, the railroad yard, and the highway system. Access to an efficient transportation network is important for the movement of goods around the city and state.

Anchorage 2020 designates Arctic Boulevard as a transit-supportive development corridor. According to Anchorage 2020, a transit-supportive development corridor represents “optimal locations for more intensive commercial and residential land use patterns which will support and encourage higher levels of transit service.”

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#### What is Anchorage 2020?

The document *Anchorage 2020: Anchorage Bowl Comprehensive Plan*, adopted by the Municipal Assembly on February 20, 2001, is a blueprint for development in the Anchorage Bowl through the year 2020.

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# West Dowling Road Connection Project

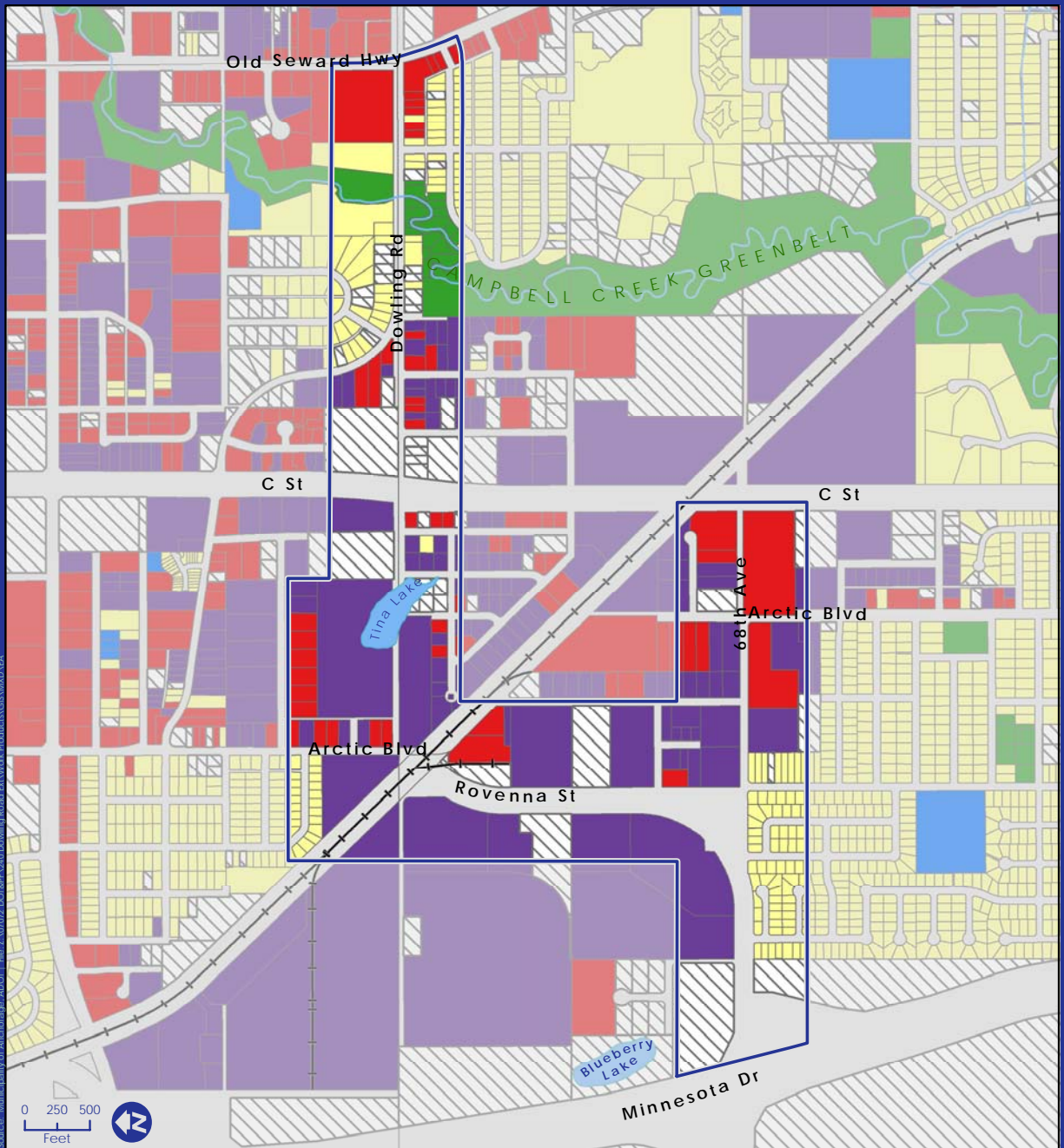


Figure 3.21-20  
Land Use

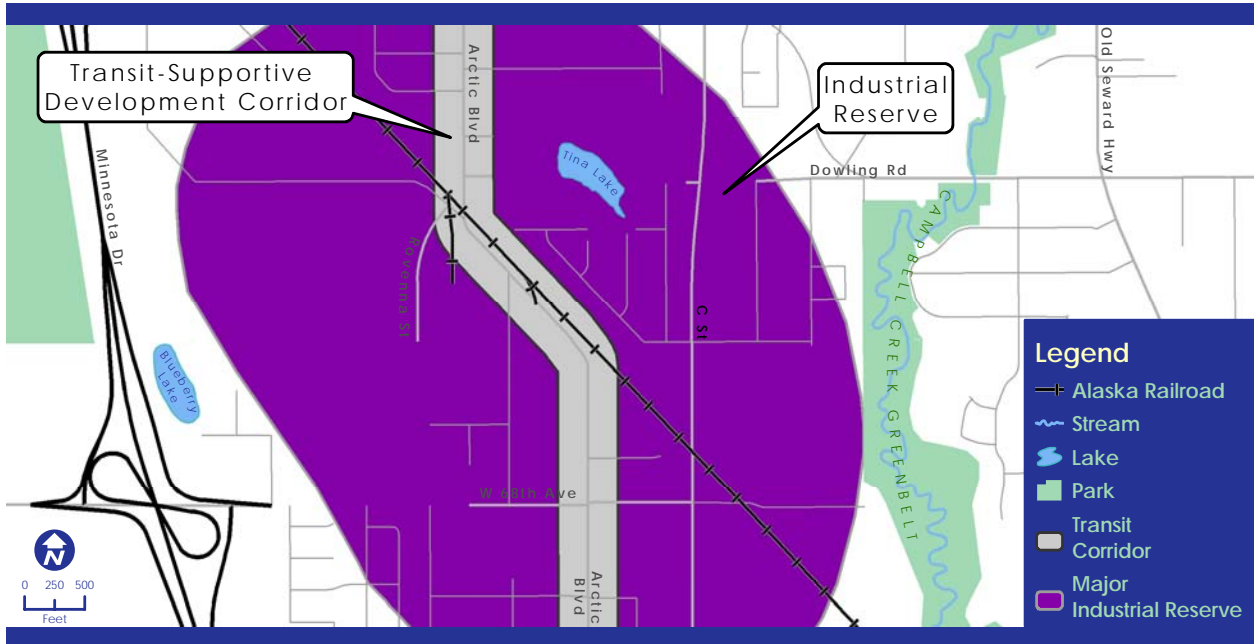


Figure 3.2224 Anchorage 2020 Land Use

The existing zoning in the study area allows for residential, industrial, commercial, and public lands and institutions. Figure 3.2322 shows the zoning designations in the study area. Table 3.4 describes the zoning codes.

Table 3.4. Zoning

Description	Designation
Local and Neighborhood Business District	B-1A
General commercial uses	B-3
Light manufacturing and wholesale	I-1
Light manufacturing and wholesale with special limitations	I-1 SL
Heavy industrial	I-2
Public lands and institutions	PLI
Public lands and institutions - park	PLI-p
Single-family residential	R-1
Two-family residential (large lot)	R-2A
Multi-family residential	R-2M
Urban and suburban single-family, two-family, and multi-family residential	R-3
Urban and suburban single-family, two-family, and multi-family residential with special limitations	R-3 SL
Transition District	T

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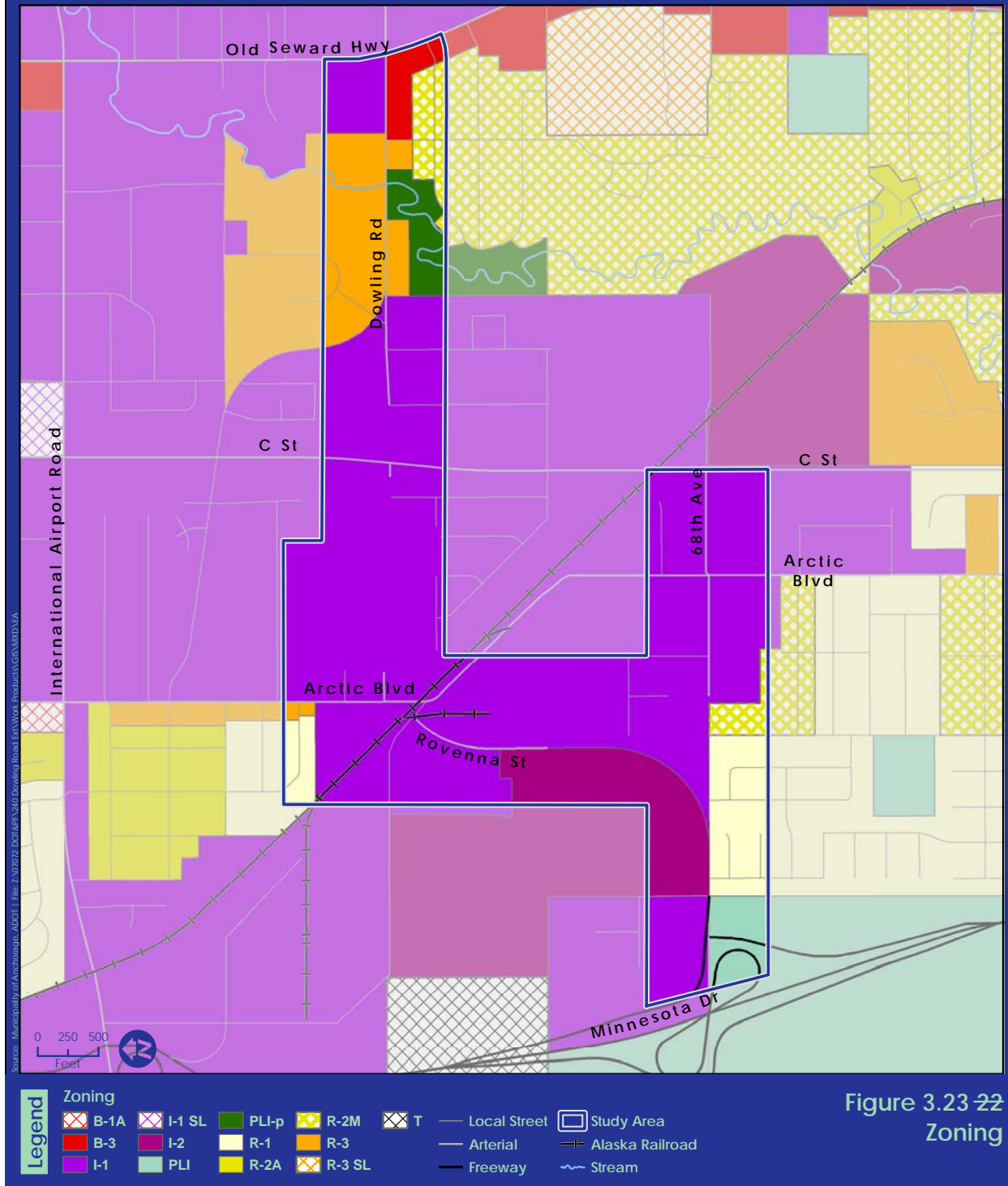


Figure 3.23-22  
Zoning

## Environmental Consequences

The No Action Alternative would have no change on existing land use patterns and zoning in the study area. However, long-term negative effects on land use would occur without the project. It would not help implement Anchorage 2020. The industrial reserve (shown in Figure 3.2224) would continue to lack adequate connections to Minnesota Drive and New Seward Highway. The lack of connectivity could limit the growth of the industrial reserve.

The Proposed Action would ~~have~~ cause direct conversion of land uses through the construction of the roadway. Most of the proposed corridor has already been reserved as a road corridor in anticipation of this project. The ROW acquired would come primarily from residential and industrial uses. Because the project is anticipated in the comprehensive plan and long range transportation plan it is consistent with those plans and benefits their implementation. The land use impacts and planning implications are discussed in more detail below.:-

The ROW acquired from the industrial users would take that land out of use for industrial purposes. The ROW acquisition would slightly reduce the amount of industrial land in the industrial reserve. However, improved access to the industrial area would make it easier for industrial users to transport goods and benefit the industrial reserve. The proposed project is expected to have a minimal impact of the availability of industrial land. Inside the Industrial Reserve, there are approximately 244 acres of vacant land. Other land within the reserve could also be redeveloped to accommodate additional industrial uses.

The Proposed Action is consistent with Anchorage 2020. The Proposed Action would benefit the industrial reserve by providing additional access to and from Ted Stevens Anchorage International Airport and National Highway System. The Proposed Action would help reduce traffic congestion, pollution, and noise by reducing trip lengths.

The Proposed Action would help meet the MOA goals of sustaining parks, trails, and recreation by including sidewalks, pathways, and improvements to the Campbell Creek Trail. These trail connections would help implement the MOA *Areawide Trails Plan* (1997) which calls for a grade-separated path under Dowling Road. These features would also improve access to the Arctic Boulevard transit-supportive development corridor that connects with adjacent neighborhoods.

### **Construction**

Because construction staging areas would be located within the ROW, no construction impacts are anticipated.

### **Mitigation and Authorizations**

No mitigation is needed and no authorizations would be required.